
I-66 Transit/TDM Plan Update

CTB Workshop – December 10, 2019

Jennifer DeBruhl, Chief of Public Transportation
Department of Rail and Public Transportation



Virginia Department of Rail and Public Transportation

I-66 Transit/TDM Plan Update

- Goal of the I-66 project is to transform Northern Virginia's Interstate 66 into a multimodal corridor that moves more people, provides reliable trips and offers new travel options
- Original strategies were developed 2014-2016 with the intent to update prior to initial implementation, based on updated assumptions



Initial Document

MAY 2016



UPDATED Document

OCT 2019

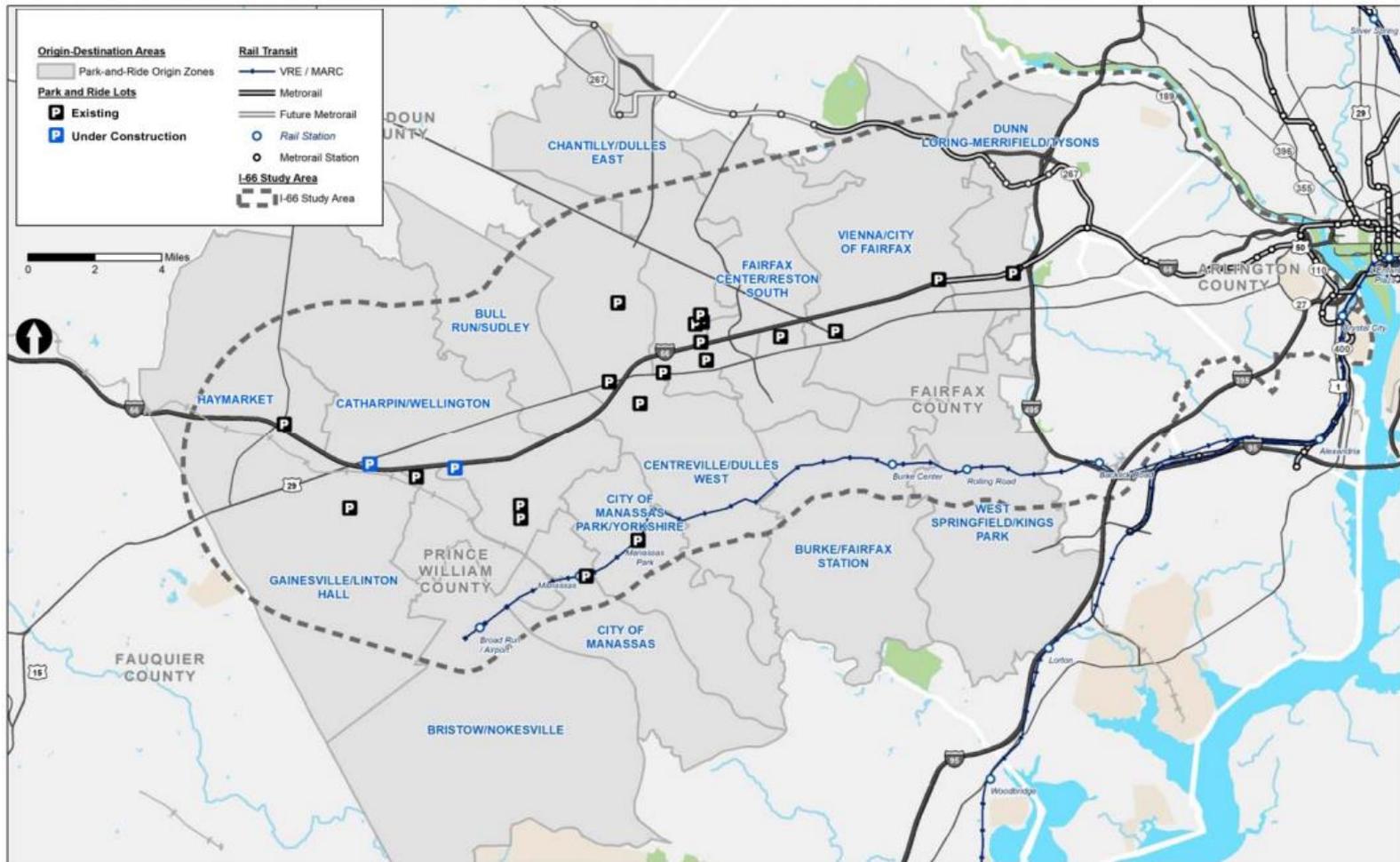
What Are The Goals?

- Increased mobility and maximize person throughput in the corridor through the identification of new transportation alternatives, including transit and TDM service improvements
- Coordination of projects that are funded by the two funding mechanisms, ***I-66 Commuter Choice*** and ***Transform 66 Outside the Beltway*** to achieve efficiency and reliability of travel along the corridor
- Evaluate the future mix of transit strategies to increase travel options and intermodal connectivity, as well as, reduce congestion in the corridor

What is different?

- Extended the study corridor to include I-66 Inside the Beltway
 - Impact of I-66 Commuter Choice on multimodal options in the corridor
 - Partnership with NVTC to coordinate services that could be funded with one or both funding programs
- Updated base assumptions with more current information –
 - Newer regional model – updated land use and travel patterns
 - Ability to support expanded commuter rail service
 - Information from the implementation of projects through I-66 Commuter Choice

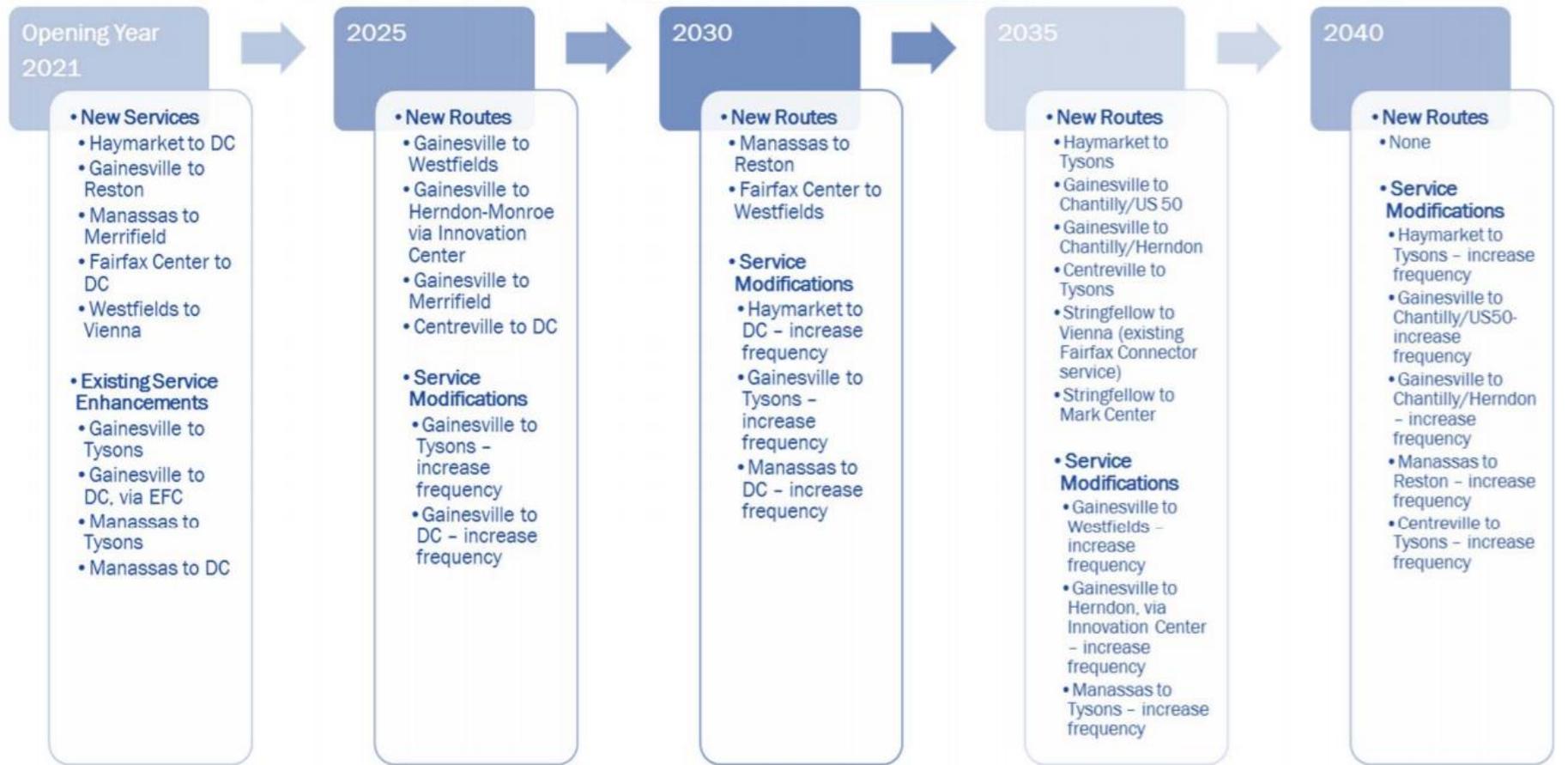
Study Area



The I-66 Corridor in this plan extends from Haymarket on the west to Washington, D.C. in the east.

Service Recommendations from the Previous Plan

Transit Recommendations From Previous Plan



Recommendations moved about 900,000 people by bus/TDM annually in 2030

Draft Service Recommendations

Draft Transit Recommendations

Route (Origin/Destination)	2022 Service			2030 Service			2045 Service		
	Headway	Peak Trips	Annual Ridership	Headway	Peak Trips	Annual Ridership	Headway	Peak Trips	Annual Ridership
Stringfellow-Pentagon	10	54	268,500	5	106	632,000	5	116	810,000
Gainesville-Pentagon	30	12	90,000	15	16	141,000	15	16	171,000
Manassas-Tysons	20	16	112,400	13	46	394,000	13	36	367,000
Stringfellow-Tysons	10	48	242,600	6	88	530,000	5	106	741,000
Stringfellow-L'Enfant Plaza	16	34	169,000	8	60	353,000	10	52	364,000
Haymarket-Ballston/Rosslyn	40	8	60,900	40	8	75,000	40	10	99,000
Manassas-L'Enfant Plaza (Downtown DC in 2030)	30	17	125,900	15	42	373,000	15	34	350,000
Gainesville-L'Enfant Plaza (Haymarket-Downtown DC in 2030)	35	14	104,600	15	38	330,000	20	26	259,000
Fairfax Center-Downtown DC	20	25	122,500	8	62	372,000	10	56	383,000
Manassas-Reston	20	20	142,300	16	34	290,000	16	30	304,000
Fairfax Center-East Falls Church	20	16	78,200	16	18	113,000	20	16	109,000
Gainesville-Tysons (Haymarket in 2045)	40	8	51,200	20	18	163,000	20	24	237,000
TOTAL BUS		272	1,568,100		536	3,766,000		522	4,194,000
VRE Manassas Line (Broad Run - Union Station)			1,963,500			4,090,200			5,084,700
TOTAL BUS & RAIL			3,531,600			7,856,200			9,278,700

Annual Ridership
 3.7 million bus riders
 4 million rail riders
 In 2030

Draft Transit Recommendations – Currently funded by I-66 Commuter Choice

Commuter Bus Route (Origin-Destination)	Service Provider	2022 Service		2030 Service		2045 Service	
		Headway (Peak Trips)	Annual Ridership	Headway (Peak Trips)	Annual Ridership	Headway (Peak Trips)	Annual Ridership
Stringfellow-Vienna Metro-Pentagon	Fairfax Connector	10 min 54 trips	268,500	5 min 106 trips	632,000	5 min 116 trips	810,000
Gainesville-Pentagon	OmniRide	30 min 12 trips	90,000	15 min 16 trips	141,000	15 min 16 trips	171,000
Stringfellow-L'Enfant Plaza (DC)	Fairfax Connector	16 min 34 trips	169,000	8 min 60 trips	353,000	10 min 52 trips	364,000
Haymarket-Ballston/Rosslyn	OmniRide	40 min 8 trips	60,900	40 min 8 trips	75,000	40 min 10 trips	99,000
Gainesville-L'Enfant Plaza (DC) <i>(Extend to Haymarket in 2030)</i>	OmniRide	35 min 14 trips	104,600	15 min 38 trips	330,000	20 min 26 trips	259,000
Fairfax Center-Downtown DC	Fairfax Connector	20 min 25 trips	122,500	8 min 62 trips	372,000	10 min 56 trips	383,000
Gainesville-Tysons <i>(Extend to Haymarket in 2045)</i>	OmniRide	40 min 8 trips	51,200	20 min 18 trips	163,000	20 min 24 trips	237,000
TOTAL BUS			866,700		2,066,000		2,323,000

Draft Transit Recommendations for I-66 Outside the Beltway funding

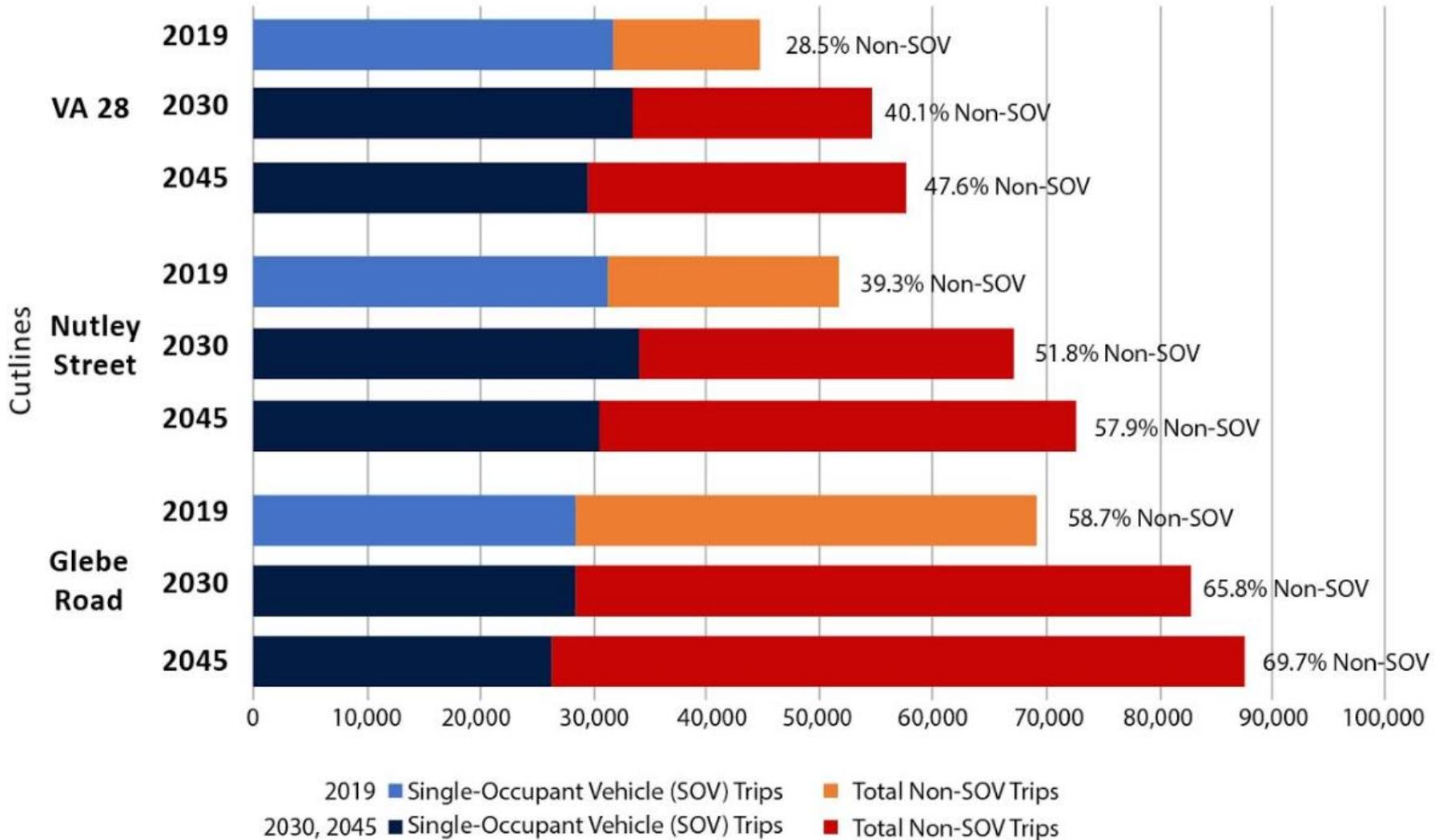
†††

Commuter Bus Route (Origin-Destination)	Service Provider	2022 Service		2030 Service		2045 Service	
		Headway (Peak Trips)	Annual Ridership	Headway (Peak Trips)	Annual Ridership	Headway (Peak Trips)	Annual Ridership
Manassas-Tysons	OmniRide	20 min 16 trips	112,400	13 min 46 trips	394,000	13 min 36 trips	367,000
Stringfellow-Tysons	Fairfax Connector	10 min 48 trips	242,600	6 min 88 trips	530,000	5 min 106 trips	741,000
Manassas-L'Enfant Plaza (DC) <i>(Extend to Downtown DC in 2030)</i>	OmniRide	30 min 17 trips	125,900	15 min 42 trips	373,000	15 min 34 trips	350,000
Manassas-Reston	OmniRide	20 min 20 trips	142,300	16 min 34 trips	290,000	16 min 30 trips	304,000
Fairfax Center-East Falls Church Metro	Fairfax Connector	20 min 16 trips	78,200	16 min 18 trips	113,000	20 min 16 trips	109,000
TOTAL BUS			701,400		1,700,000		1,871,000
VRE Manassas Line <i>(Broad Run-Union Station)</i>	Virginia Railway Express		1,963,500		4,090,200		5,084,700
TOTAL BUS & RAIL			2,664,900		5,790,200		6,955,700

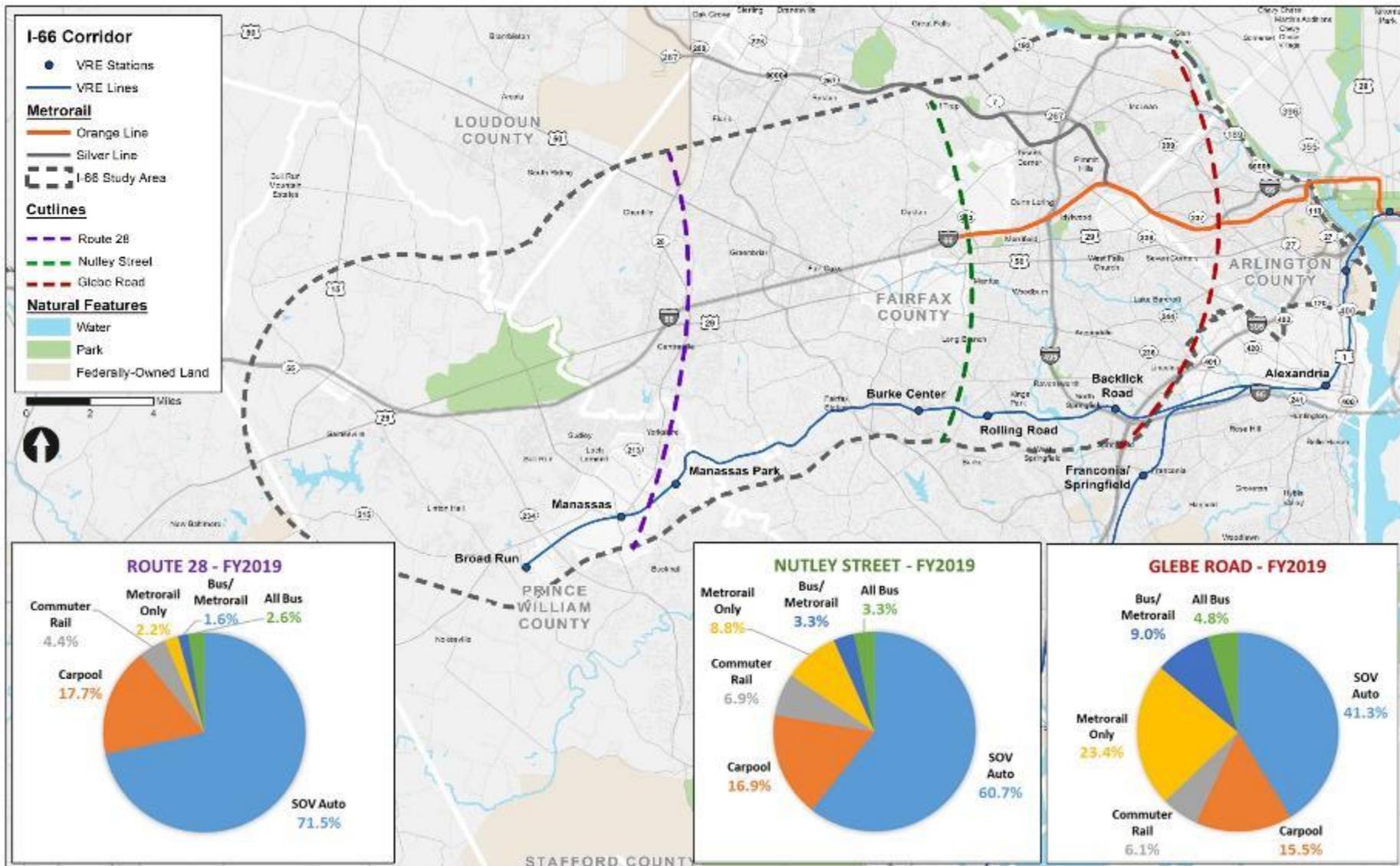
Impact of Draft Recommendations

Persons Moved on Eastbound I-66

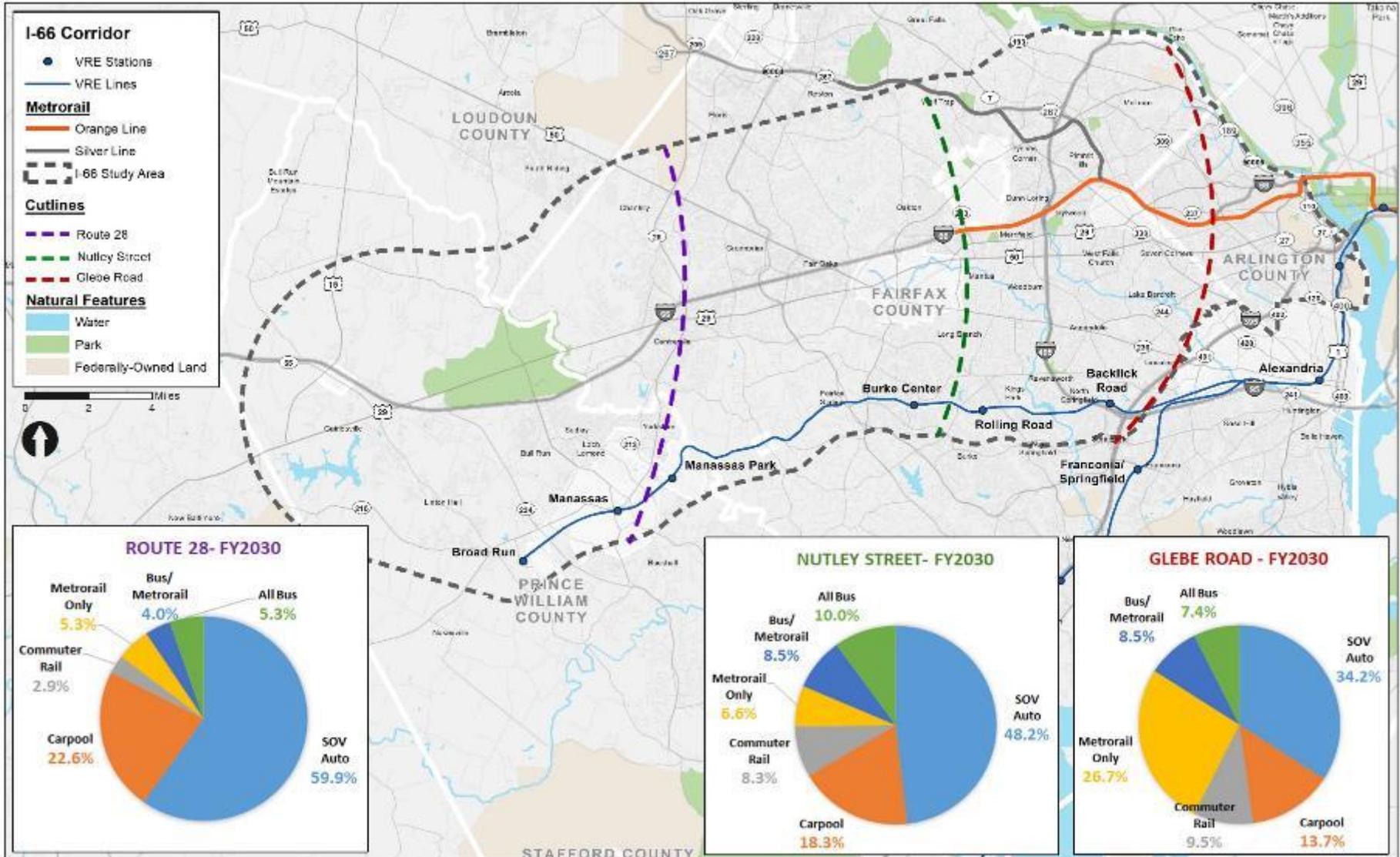
Current (2019) and Future Conditions with Investment (2030, 2045)



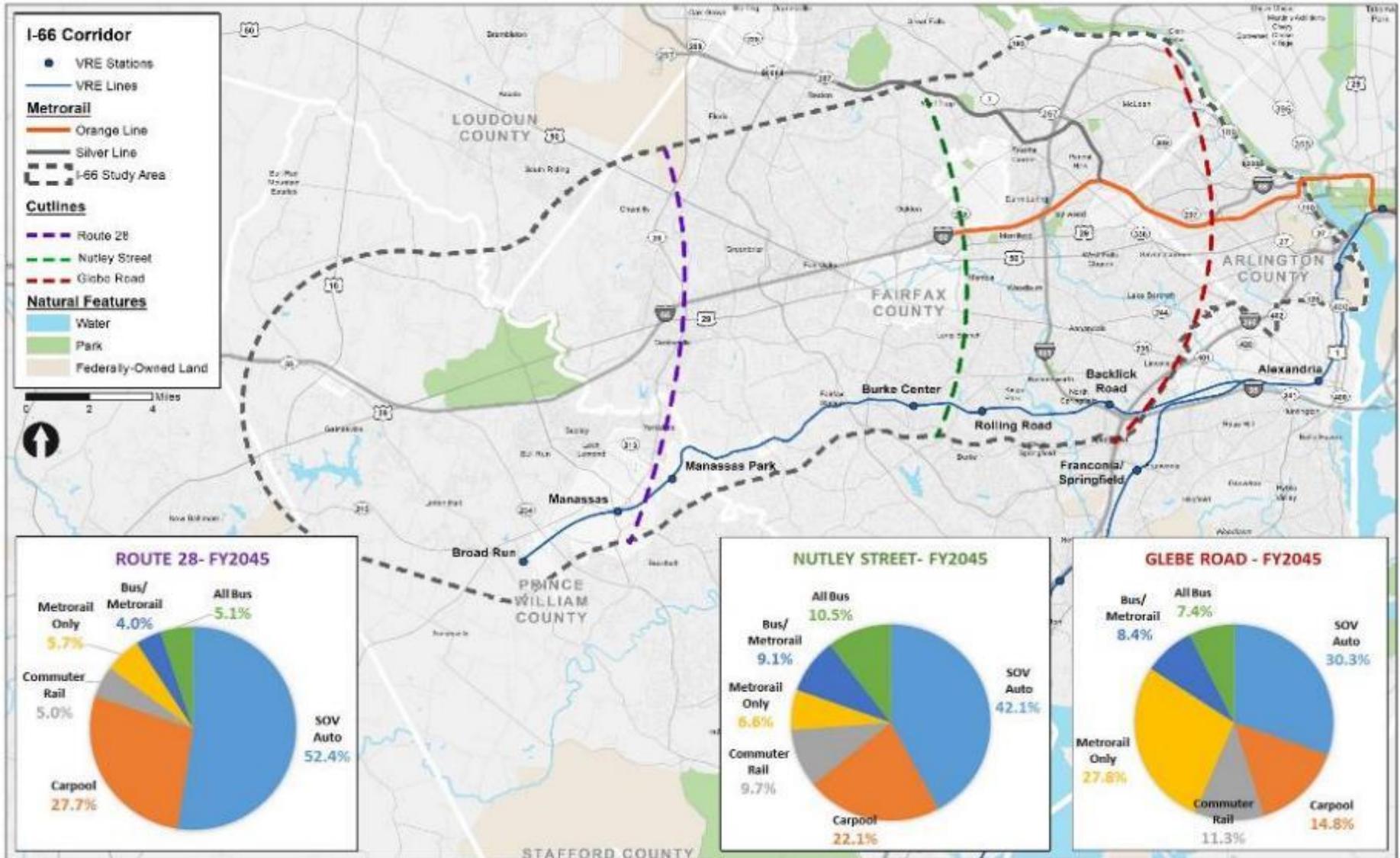
Mode Split - 2019



Mode Split - 2030



Mode Split - 2045



Impact of Revised Recommendations on Person Throughput

Annual Peak Period Ridership: Draft Transit Plan			
	2022 Service	2030 Service	2045 Service
Total Bus Service	1,568,100	3,766,000	4,194,000
Total Rail (VRE Manassas Line)	1,963,500	4,090,200	5,084,700
Total Bus & Rail	3,531,600	7,856,200	9,278,700

Annual Peak Period Ridership: Previous Plan*		
	2025 Service	2040 Service
Bus Service Recommendations	813,120	986,040

*Daily peak period ridership was used in previous plan and has been converted to annual peak period ridership

Next Steps

- Completion of plan/coordination with stakeholders
- Service demands change over time – continual need for evaluation and adjustment of services
- Availability of technology to assist in first/last mile solutions will evolve over time – RM3P and other projects will inform future services

I-495/American Legion Bridge Transit/TDM Study

I-495/American Legion Bridge Transit/TDM Study

The American Legion Bridge provides the only direct connection between the region's most populous counties.

The goal of the I-495/American Legion Bridge Transit/TDM Study is to:

Identify a range of current and future multimodal solutions that can be implemented to reduce highway and transit congestion and improve overall mobility within the corridor.



Study Process



- Evaluate forecasted changes in land use, population, households, and employment
- Review existing and projected travel patterns
- Identify opportunities to move more people through transit and transportation demand management
- Develop optimized slate of multimodal recommendations

Next Steps

- Identify and coordinate with stakeholders
- Finalize project scope and secure consultant resources
- Kick-off meeting – early 2020
- Align transit/TDM recommendations with project development milestones

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